



PROJECT WILDCAT JUNE 2025

EXECUTIVE SUMMARY



INTRODUCTION

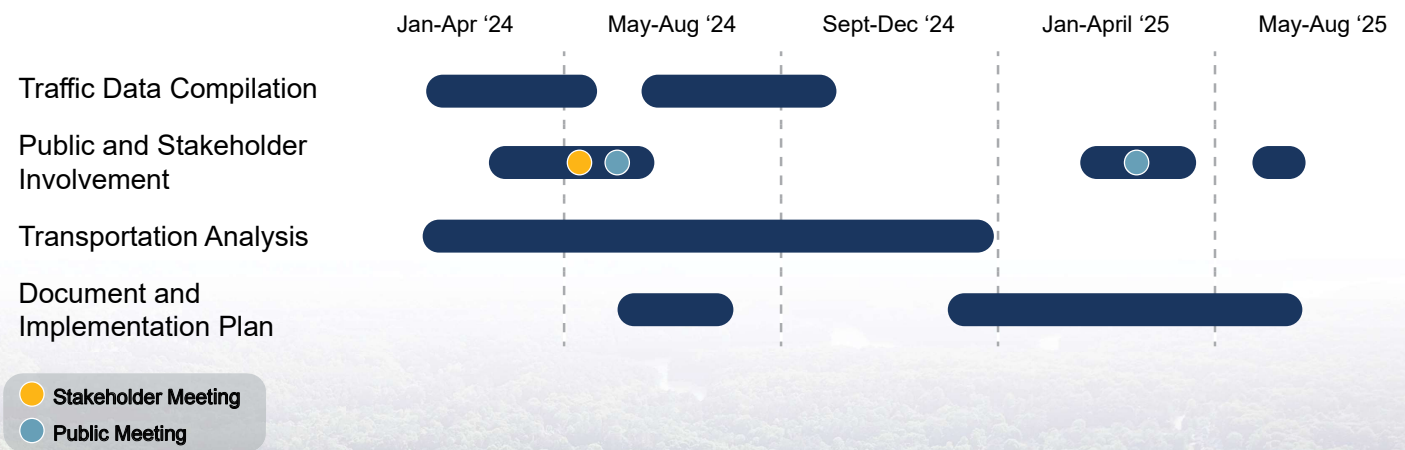
Cherokee County, the Cherokee Office of Economic Development, and the cities of Woodstock and Holly Springs, partnered to conduct a small area plan and transportation study at the I-575 and Sixes Road interchange. The study area includes the southeast corner of the interchange that is bounded in part by the Lebanon United Methodist Church Cemetery to the north, Army Corps of Engineers (USACE) property for Lake Allatoona to the south, Interstate 575 to the west, and the Georgia Northeastern Railroad to the east. This area is known from its historic roots as “Wildcat.”

The interchange area has been well-positioned for success due to its strategic location near I-575 while also providing important access for Bells Ferry Road, Holly Springs Parkway/Main Street, and East Cherokee Drive. Despite significant investments in roadway infrastructure in and around Cherokee County, the study area has remained constrained by listed considerations, which has limited its potential for growth and connection.

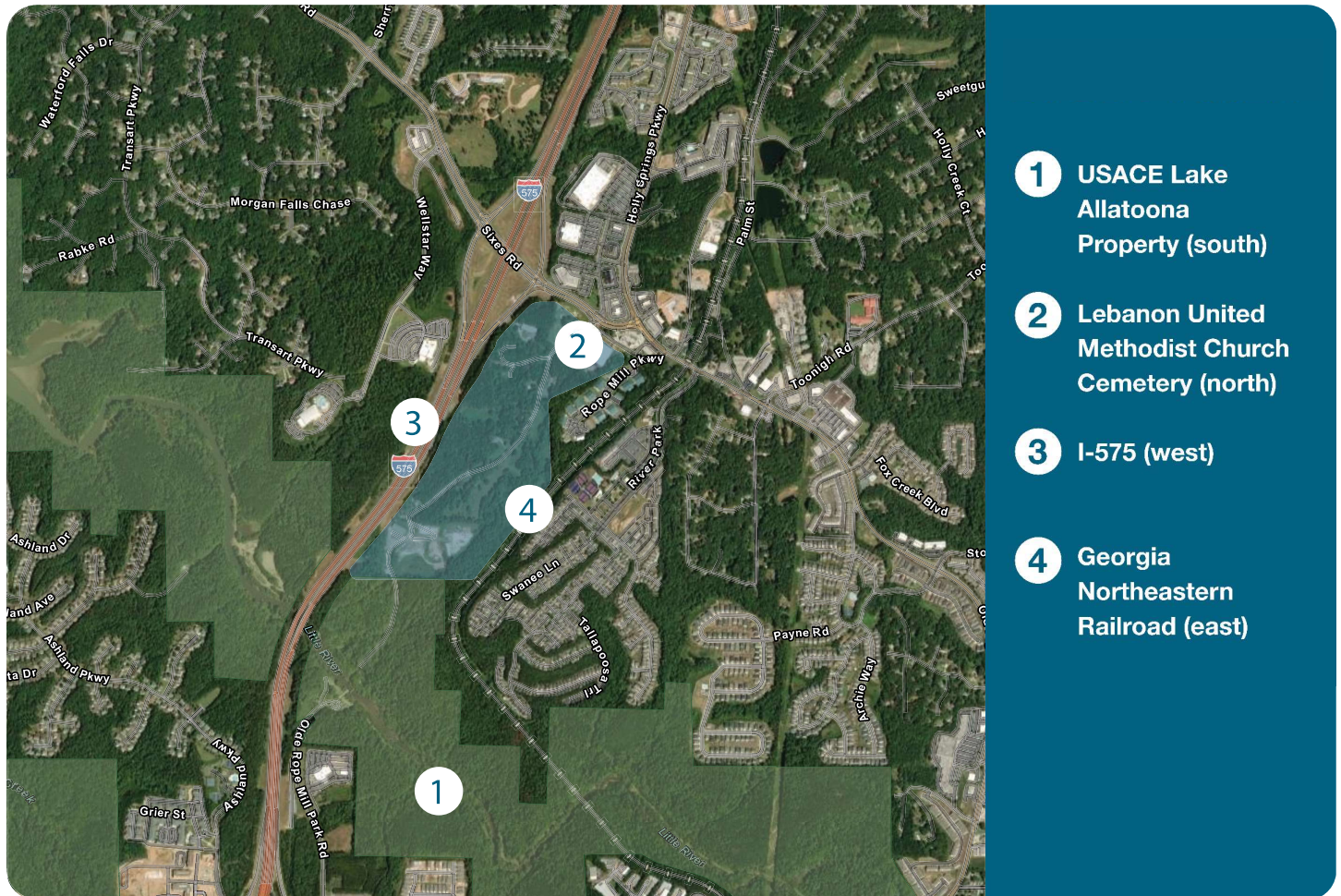
The goal of this study was to identify potential transportation network recommendations that could improve the current operation of the roadway by reducing congestion and increasing safety while also providing meaningful connectivity to support future growth and development around the interchange. This summary provides an overview of the study process and recommended strategies.

The study began in early 2024 with efforts focused on data collection and baseline transportation analysis. The first round of outreach included a request to the public for their insights on the transportation and development needs of the area, which informed recommendations. Following an extensive traffic analysis and complementary geometric engineering and environmental assessment, the draft recommendations were vetted with the public in the spring of 2025.

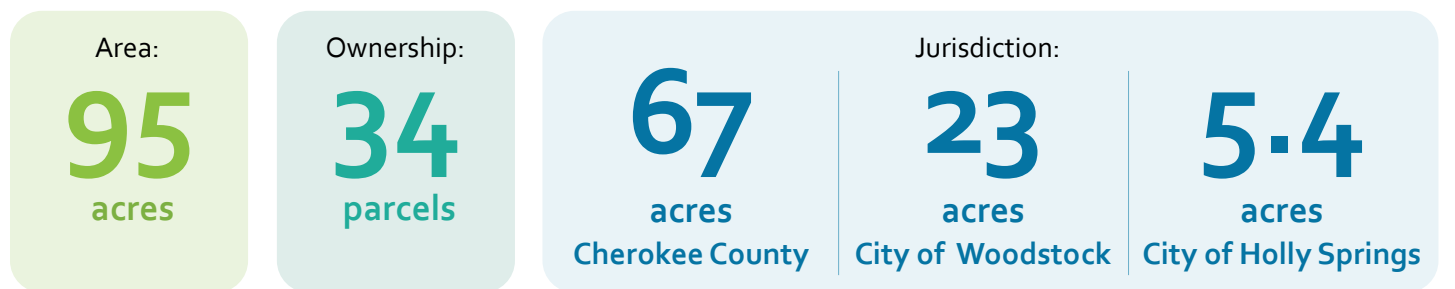
Project Schedule



STUDY AREA



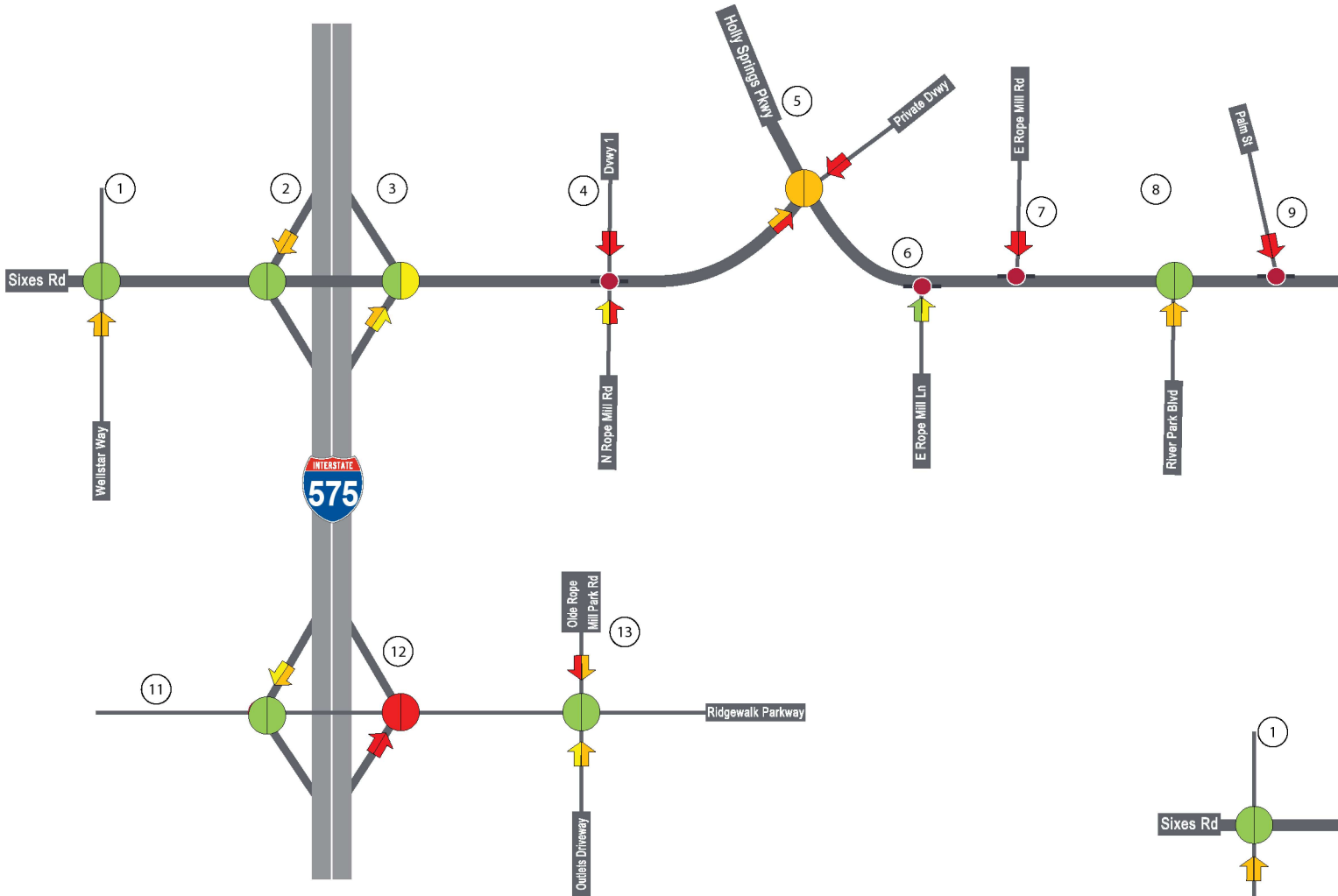
Subarea Development Stats:



The subarea is well positioned in a growing area of Cherokee County and the Region:

	Population		Jobs	
	2020	2050	2020	2050
Within 5 miles	125,000	231,000	40,000	55,000
Within 10 miles	417,000	576,000	170,000	212,000

CURRENT AND FUTURE TRAFFIC CONSIDERATIONS



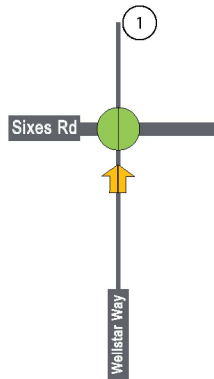
Cherokee 575 Sixes Study Area

Existing (2024) Peak Hour Traffic Volumes

Existing Traffic Conditions

As it stands today, roads in the study area are congested due to unusual roadway geometry and high turning volumes. Traffic modeling shows:

- Congested conditions at the Sixes Road and Holly Springs Parkway intersection
- Failing approaches on side streets at intersections along Sixes Road and Holly Springs Parkway



11



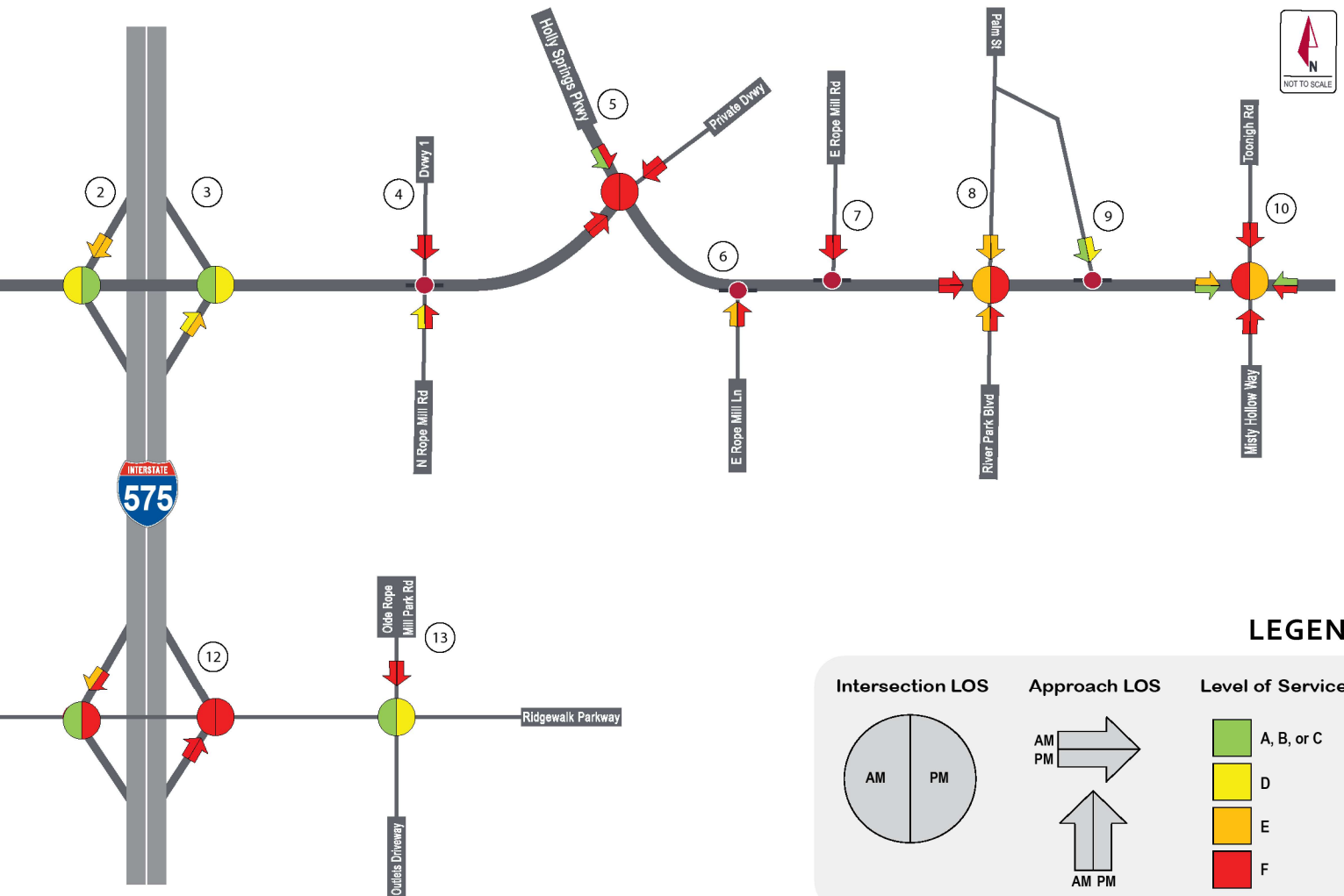
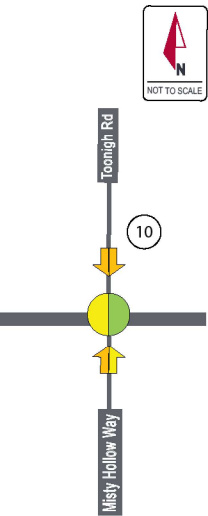
Cherokee 575 Sixes Study Area

No-Build (2044) Peak Hour Traffic Volumes

No-Build (2044) Traffic Conditions

If no new improvements are made to roads in the study area, congestion will worsen at all intersections by 2044. Traffic modeling shows:

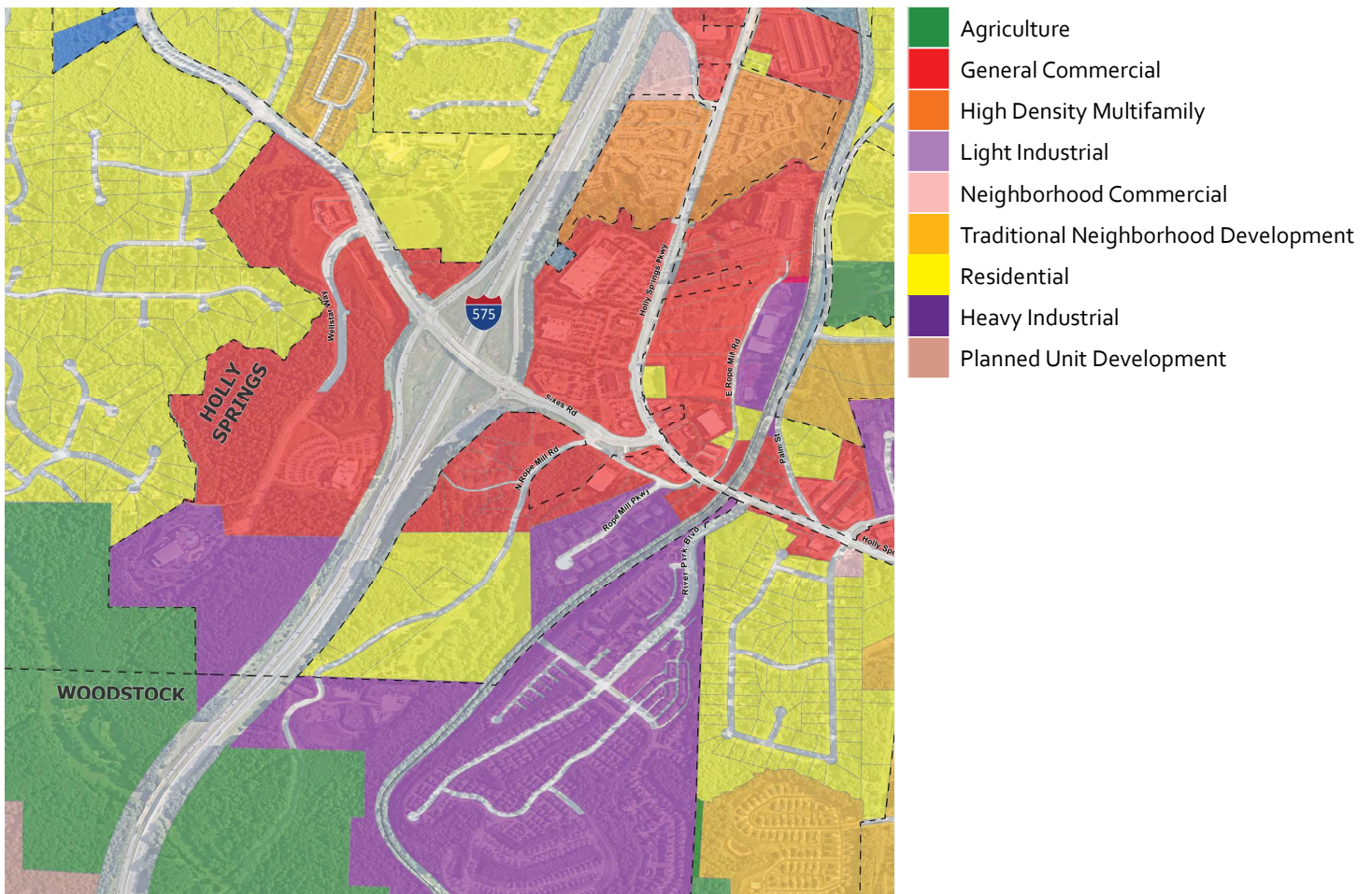
- Extreme delays at the Sixes Road and Holly Springs Parkway intersection
- Complete breakdown of flow along Holly Springs Parkway, with multiple failing intersections in both peak hours.
- Worsening conditions at the I-575 and Sixes Road interchange



DEVELOPMENT OPPORTUNITIES

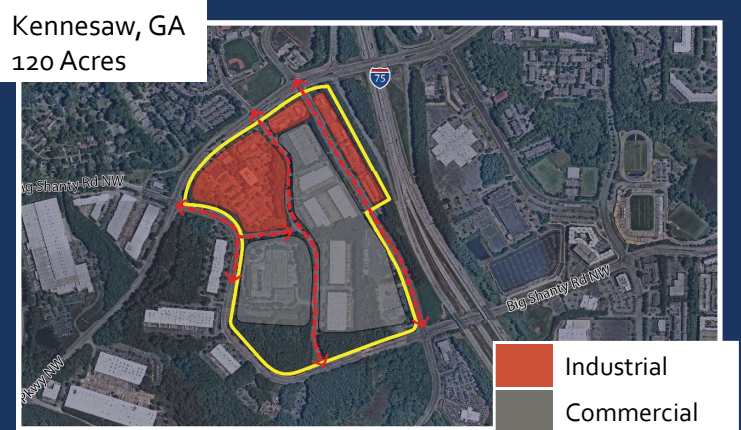
As a part of the potential development assessment, the study team inventoried the existing and future land use as well as zoning of the Wildcat and surrounding areas to inform what was currently possible. Additionally, the team explored case studies of similar properties around the southeast to compare what had been developed on similar style sites.

The eventual land use scenario shown in the map on the right was created based on two criteria: 1) how much development can physically fit on the land and 2) how much new traffic the proposed roadway network can support. The possible land use types included around the interchange area are light industrial, medical offices, and an office park, which may include a hotel and retail spaces. Additionally, development on both sides of the interstate can include thoughtful incorporation of greenspace. This land use/development scenario was used for the purposes of studying traffic in the Wildcat area, but the final development plan and uses have not been determined.



Case Studies

Case studies were selected based on their proximity to an interstate or highway, their comparable acreage to the study area off I-575, and variety of uses. These examples were utilized to identify the most suitable development scenario for the project area.





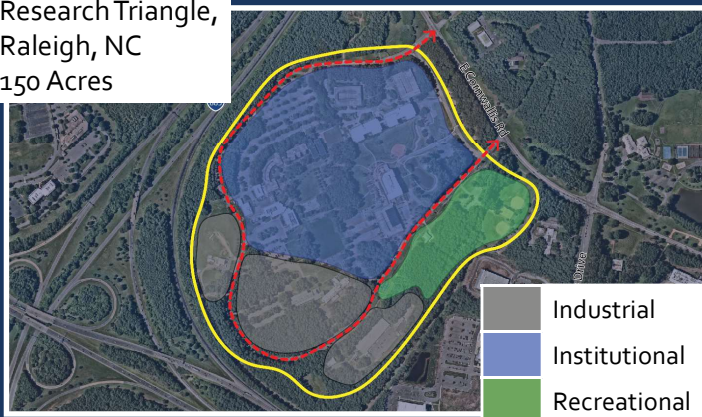
East of I-575	
Office Park	1,000,000 sqft
Retail	250,000 sqft
Hotel	100 rooms
Total	1,250,000 sqft*
West of I-575	
Medical Office Building	300,000 sqft
Light Industrial	250,000 sqft
Total	600,000 sqft

Total Density	
Office Park	1,250,000 sqft
Retail	250,000 sqft
Hotel	100 rooms
Medical Office Building	300,000 sqft
Light Industrial	250,000 sqft
Total	1,850,000 sqft*

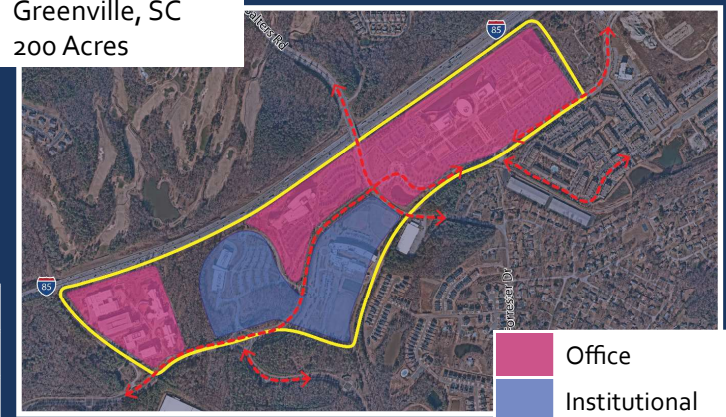
**Square footage from hotel not included*

Final development plan has not been determined

Research Triangle,
Raleigh, NC
150 Acres



Greenville, SC
200 Acres



TRANSPORTATION NETWORK RECOMMENDATIONS

This plan used extensive traffic modeling to test and evaluate transportation improvements in the Sixes Road area. The below recommendations were developed to satisfy the goals of reducing congestion, supporting new interchange development, and planning for multimodal connections.

ID	Project Name	Category	Description
I-1	Holly Springs Pkwy and Sixes Rd Realignment	Intersection Improvements	Realign Holly Springs Parkway and Sixes Road to a 90-degree intersection
NC-1	I-575 Crossing – Wellstar Way Bridge	New Connection	Construct a new, 4-lane connection between Wellstar Way at the Cherokee County Aquatic Center and N Rope Mill Road, including a new bridge over I-575
W-1	Wellstar Way Improvements	Widening	Widen and reinforce Wellstar Way to be a 4-lane road
NC-2	N Rope Mill Rd Reconstruction	New Connection	Reconstruct N Rope Mill Road to be a 4-lane road with fewer curves. Realign near Sixes Road to reduce skew and curve near intersection
W-2	Commercial Drive Improvements	Widening	Widen, reinforce, and stripe the commercial drive at the Sixes Road and N Rope Mill Road intersection to be 4-lanes; includes a potential roundabout at the Home Depot driveway
W-3	Holly Springs Parkway Widening Phase I	Widening	Widen Holly Springs Parkway from Sixes Road to E Cherokee Drive from 2 lanes with a two-way left-turn lane to 4 lanes with a median and dedicated left-turn lanes where appropriate
W-4	Holly Springs Parkway/Main Street Widening Phase II	Widening	Widen Holly Springs Parkway/Main Street from E Cherokee Drive to Ridgewalk Parkway from 2 lanes with a two-way left-turn lane to 4 lanes with a median and dedicated left-turn lanes where appropriate
NC-3	Ridgewalk Parkway Arterial Access Lanes	New Connection	Construct a pair of one-way access lanes between Ridgewalk Parkway and Wellstar Way Bridge; Access lanes connect via Olde Rope Mill Rd northbound and connect to intersection west of I-575 southbound
NC-4	I-575 Slip Lanes	New Connection	Construct a pair of one-way “slip” lanes between I-575 near Ridgewalk Parkway and Wellstar Way Bridge; The slip lanes allow drivers to exit the interstate and go directly to Wellstar Way Bridge without using the interchange; The slip lanes will merge with the access lane system near Wellstar Way Bridge
NC-5*	River Park Neighborhood Connector	New Connection	Potentially construct a two-lane new connection between N Rope Mill Road and Wellstar Way Bridge to Georgia Way

*NC-5 only to be constructed with additional neighborhood input with City of Woodstock approval